

PUBLIC RIGHT-OF-WAY **PERMITTING AND CONSTRUCTION MANUAL**

TABLE OF CONTENTS

1.	INTRODUCTION	2
2.	CONSTRUCTION PERMITTING PROCEDURE	2
2.1	Permitting Process	2
2.2	Permit Application	3
2.2.1	Permits on Site	3
2.3	Construction Plan	3
2.4	Storm Water Pollution Prevention Plan	4
2.5	Traffic Control Plan	5
2.6	Trench Safety Plan	5
3.	CONSTRUCTION REQUIREMENTS	6
3.1	Notification to the City and Work Hours	6
3.2	Notification to the Public	7
3.3	Existing Facility Locates	7
3.4	Street Cuts and Excavation	10
3.5	Installation	10
3.6	Backfill	14
3.7	Restoration	15
3.8	Exceptions	16
3.9	Miscellaneous	16

ATTACHMENTS

Permanent Backfill and Repair Detail

1

INTRODUCTION

Because of the increasing number of facilities in the public right-of-way, the City has adopted the Right-of-Way Management Chapter of the City's Code of Ordinances ("Right-of-Way Ordinance" or "Ordinance") and developed this manual in order to guide and manage the use of the public right-of-way. This manual is intended to provide technical criteria and details necessary to implement the provisions of the Right-of-Way Ordinance.

2

CONSTRUCTION PERMITTING PROCEDURE

2.1 Permitting Process

1. Prior to obtaining a permit to perform construction within the public right-of-way, a public service provider must first register with the City of Arlington in accordance with Article IV of the Right-of-Way Ordinance, using the registration form prescribed by the City of Arlington. Additional information regarding registration for construction within the public right-of-way may be obtained by contacting the Review Engineer in the Economic Development Services Department, Right-of-Way Division at (817) 459-6634 or by e-mail at rowpermits@arlingtontx.gov.
2. In addition, all public service providers, as well as their prime contractors and sub-contractors must be a registered user at www.arlingtonpermits.com.
3. Prior to performing construction within the public right-of-way, the public service provider, or its authorized representative, is required to obtain a permit from the City in accordance with Article V of the Right-of-Way Ordinance.
4. All new submittals for a permit application shall be filed using www.arlingtonpermits.com and contain a completed digital file of the plans in .pdf format along with the approved traffic control plans (TCP) if traffic lanes are proposed to be closed. In addition, a storm water pollution prevention plan and trench safety plan may also be required. All submittals shall be completed in accordance with this Manual.
5. Revised plans addressing review comments shall be resubmitted with a copy of the original permit application and shall be clearly marked as "resubmittal".

2.2 Permit Application

1. The permittee is required to complete the Public Right-of-Way Permit Application. A copy of the application is included with this document.
2. Construction must begin within 90 days of permit issuance and any extension must be approved by the City in writing. Otherwise, the permittee will need to resubmit a new permit application.
3. For any work within the State right-of-way, the public service provider shall provide evidence of a permit from the State within one week of receipt of permit. In all cases, evidence of a permit must be provided a minimum of 48 hours prior to construction.

2.2.1 Permits on Site

During construction, all contractors shall have all permits and plans associated with the work present on site and shall be made available to the City right-of-way inspector or other City staff upon request.

2.3 Construction Plan

The construction plans shall show the following:

1. The location of the facility being permitted.
2. The limits of the proposed work.
3. The location of all existing and proposed public facilities, including City water lines, storm drainage facilities, and sanitary sewer lines in relation to all proposed utilities, if there is a potential for conflict. Construction drawings for existing and proposed public facilities may be requested or viewed in the City of Arlington Map Room in person on the first floor of City Hall or via e-mail at maproom.water@arlingtontx.gov. The elevation of the existing and proposed public facilities shall be noted, the size or diameter of pipe, a profile shown for any crossing over or under City facility, in relation to the proposed utility line, if there is a potential for conflict. The plans shall indicate how potential conflicts will be avoided.
4. Detail of proposed facility installation, i.e., pipe size, depth and dimensions of occupied space. If a utility structure is proposed, dimensions, type, and location shall be indicated on the plans.
5. Pavement removal and replacement limits for street cuts, when allowed.
6. The length and depth of all bores.

- A. All concrete driveways and streets shall be bored. The length of the bore must be sufficient for meeting the fully improved (ultimate) roadway width as specified in the City of Arlington Thoroughfare Development Plan, unless right-of-way to do so is not available.
- B. Pavement cuts in newly constructed, reconstructed or resurfaced (greater than one inch) asphalt streets shall only be allowed within 60 months after the completion of the street work as provided by Right-of-Way Ordinance § 5.06(C), as amended.
- 8. Detailed drawings of any bores, trenches, handholes, manholes, vaults, switch gears, transformers and pedestals, including depth.
- 9. The location of all trees and shrubs within the right-of-way and the landscape protection measures.
- 10. Complete legend of drawings.

Exceptions may be requested in accordance with the Ordinance.

2.4 Storm Water Pollution Prevention Plan

The permittee shall submit two sets of a Storm Water Pollution Prevention Plan in cases where stream/creek crossings are open cut. A four-foot vertical clearance below the bottom of the proposed stream bed or drainage facility is required. The permittee shall contact the Department of Public Works and Transportation Stormwater Division for future improvements to the stream/creek, which may impact the proposed alignment.

In all other cases, the permittee is required to implement erosion control measures for construction activities in accordance with the Storm Water Pollution Control Ordinance of the City Code, as amended, and other City ordinances, state laws, and federal regulations. The following pollution prevention measures shall be used where applicable:

- 1. Avoid placing structural pollution prevention controls in the floodway.
- 2. Trap/contain boring “mud” or waste material to prevent flow in the street and/or storm drain system through the use of a vacuum excavator, or equivalent method.
- 3. Remove construction debris and trash daily.
- 4. Place erosion control matting, hydromulch seeding or sod on bare ground as soon as possible, but no later than 14 days after completion of construction work.
- 5. Clean sediment from streets and other paved surfaces. Sediment shall be removed by sweeping and not by washing into the storm drain system.

2.5 Traffic Control Plan

Any work that may impact traffic flow or result in lane closures in streets will require a traffic control plan and the closures shall comply with the most current edition of the *Texas Manual on Uniform Traffic Control Devices*. The permittee shall indicate on the permit application if a lane closure is required. It is the permittee's responsibility to submit and obtain approval of a site-specific traffic control plan from the Department of Public Works and Transportation Traffic Engineering Division. In addition:

1. A permittee shall not cause or allow interference with traffic flow on any street included in the Thoroughfare Development Plan during the hours of 7:00 a.m. through 9:00 a.m. and 4:00 p.m. through 6:00 p.m. Monday through Friday. If construction on a partially closed street stops for the day, all lanes must be reopened to traffic, unless an extended time of closure is expressly granted by the City.
2. All traffic control plans involving roadways on the current version of the City's Thoroughfare Development Plan, including all major and minor arterials as well as major and minor collectors, shall include an arrow board. Roadways that are part of the state highway system shall require an additional approved Traffic Control Permit from the Texas Department of Transportation.
3. When directing traffic, paddle boards indicating "stop" and/or "slow" are required. Signs indicating the closure of a sidewalk shall comply with all accessibility standards under the Americans with Disabilities Act ("ADA") or other applicable law.
4. All construction in street right-of-way within an area bound by Lamar Boulevard, Cooper Street, Abram Street, and State Highway 360 shall be completed four hours prior to the start time of any event at AT&T Stadium and Globe Life Park in Arlington. Any traffic lanes closed shall be opened and functional. Any work that may impact any Convention Center event shall be coordinated to minimize traffic conflicts and street closures.
5. Emergency Work. If a TCP is not submitted before emergency work commences, a Public Service Provider will submit a TCP to the Department of Public Works and Transportation, Traffic Engineering Division, within forty-eight (48) hours after the emergency work is completed.

2.6 Trench Safety Plan

Trench safety systems shall meet or exceed U.S. Occupational Safety and Health Administration Standards.

3 CONSTRUCTION REQUIREMENTS

3.1 Notification to the City and Work Hours

1. Once a permit is issued, permittee shall give the City Right-of-Way Inspector a minimum notice of two (2) working days prior to commencing work. Permittees may send notice via e-mail to rowinspector@arlingtontx.gov, and must include the start date, approved right-of-way permit, Traffic Control Permit, and approved construction plans.
2. Except in the case of an emergency, no work shall be permitted during the following times unless authorized in writing by the City:
 - a. During the hours of 6:00 p.m. to 7:00 a.m. Central Standard Time.
 - b. During the hours of 8:00 p.m. to 7:00 a.m. Central Daylight Savings Time.
 - c. For boring activities, during the hours of 5:00 p.m. to 8:00 a.m., unless otherwise approved by City.
3. The City may require that the work occur overnight when necessary to expedite construction and minimize disruption to traffic.
4. No work shall be permitted within the Entertainment District on the business day preceding the date of any event that qualifies as a special event under the Special Events Chapter. For example, if a special event is scheduled on Sunday, then no work shall take place on the preceding business day, Friday, unless Friday is a holiday in which case the preceding business day would be Thursday.
5. A pre-construction meeting with the City Right-of-Way inspector or other City Staff is required when construction work is proposed to occur within a two-mile radius of the Entertainment District and at least five (5) business days prior to the commencement of construction.
6. **Holiday and Weekend Work.**
 - A. Weekend work will be limited in scope as determined by the City Right-of-Way Inspector through the permitting process. No work will be permitted on holidays, as noted below, except in the most extreme circumstances and with prior approval. If approved to work on a holiday or weekend, a permittee may be subject to separate inspection fees.

- B. The City of Arlington observes the following days as City holidays: New Year's Day, Martin Luther King Jr. Day, President's Day, Good Friday, Memorial Day, Juneteenth, Independence Day, Labor Day, Thanksgiving Day, the Friday after Thanksgiving, Christmas Eve, and Christmas Day.
- 7. In the event that construction work amounts to a risk to health, safety, or welfare of the public, permittees, public service providers, and other persons shall call 911.
- 8. In the event that construction work results in damage to City facilities, permittees, public service providers, and other persons shall call 817-459-5900 immediately.

3.2 Notification to the Public

The following notification procedures apply if it is necessary to close, in whole or in part, a public right-of-way:

- 1. For any closure of a traffic lane or blocking of a sidewalk or alley lasting one (1) day or less, the person performing the work on behalf of the public service provider and their general contractors and sub-contractors shall conspicuously mark their vehicle and machinery with the company name and telephone number.
- 2. Any closure of a traffic lane or blocking of a sidewalk or alley lasting longer than one day must be identified by a 3-foot by 3-foot sign that is clearly legible to the traveling public. The sign must be posted at or in close proximity to the work site and must contain:
 - A. The name of the public service provider;
 - B. The name of the company performing the construction;
 - C. A local 24-hour contact person and telephone number; and
 - D. A description of the construction project.
- 3. The requirements above are in addition to any signs, barricades, or warning devices required by law or ordinance. The sign information listed above may be included on barricades or warning devices.
- 4. The permittee will give written notification of construction to all adjacent property occupants by conspicuously posting the notification on each adjacent property at least 72 hours before commencement of construction. The City may require alternate notification requirements for permitted construction that will last longer than two (2) weeks or if the City determines that an emergency exists.

3.3 Existing Facility Locates

- 1. Prior to construction, the permittee, public service provider, or contractor shall obtain utility locates by utilizing:

Texas One Call System (“Call Before You Dig”) 811 or 1-800-344-8377

2. Prior to construction, the permittee, public service provider, or contractor shall obtain utility locates for the City of Arlington utilities, including water and sewage facilities, streetlights, traffic signals, communication systems, and median and parkway irrigation, at least two (2) working days prior to construction by utilizing the following processes, so that locates may be marked:

The Line Locate process may be accessed through the “Ask Arlington” app or the City of Arlington website, currently located online at:

https://www.arlingtontx.gov/city_hall/departments/water_utilities/engineers_builders_and_contractors/line_locate_process

3. If Line Locates for City utilities and facilities cannot be obtained through the “Ask Arlington” app or the online process above or if Line Locates are not provided within two (2) working days, Line Locates may be obtained by calling the numbers below:

Water Utilities

Water Distribution/Wastewater Collection 817-459-5900

Department of Public Works and Transportation

Streetlights 817-459-6350

Signals/Warning Flashers 817-459-5400

Technology Services

Communication Systems 817-459-6550

Parks Department

Median and Parkway Irrigation 817-575-8308

4. Permittees, Public Service Providers, contractors, and other persons shall use the City line locate process for in this Section 3.3 for Arlington utility facilities only. All other utility facilities can be located through the Texas One Call (811) System.
5. Line Locates for other utilities and facilities, as noted below, may be obtained by calling the following numbers:

AT&T Stadium 817-892-4149

Globe Life Field and Choctaw Stadium 817-533-1972

University of Texas at Arlington (UTA) 817-272-2000

Viridian Planned Development 817-640-8535

6. Permittees, Public Service Providers, contractors, and other persons shall obtain line locates for the City utilities prior to construction for any excavation regardless of depth. City utilities include water, sanitary sewer, signals, flashers, streetlights, fiber and median/parkway irrigation. Emergency line locates may be made by calling the City emergency phone line at 817-459-5900.
7. Water and sanitary sewer service lateral lines and stormwater pipes are not identified as part of the Texas One Call (811) System. Contractors are responsible for verifying depths for water, sanitary sewer laterals, and stormwater lines.
8. Discrepancies – Locate Markings, Plans and Maps. In the event that plans or maps contain differing information about the location of utilities from onsite locate markings, Permittees, Public Service Providers, contractors, and other persons shall not perform construction in areas where any plans, maps, or locate markings display the presence of live utilities, unless and until they have verified that live utilities are not present in that area.
9. Absence of Locate Marks.
 - A. If City utilities, such as water valves, sewer manholes, fiber handholes, streetlights and signals, are present and no locate marking has been made, no boring or trenching shall take place until marked and verified. The contractor shall maintain all locate tickets and provide City Staff with this information upon request.
 - B. Regarding obtaining line locates for water, sanitary sewer, and stormwater infrastructure, it will be the responsibility of the contractor or public service provider to utilize the plans and thoroughly walk the area to locate and identify meter boxes, water valves, vaults, ground boxes and manholes to determine the location and shall pothole each. Because City utilities frequently have vertical and horizontal bends, the contractor or public service provider must use all available resources to accurately verify depth prior to crossing the utilities, such as by using a camera as alternate means to identify sanitary sewer services.
10. Facility Spacing Requirements and Tolerance Zone - Underground Blocking. It is the contractor's or public service provider's responsibility to identify all utilities, including underground City water facilities, in the area where construction will be conducted in order to confirm their location and accuracy prior to construction. The contractor or public service provider shall protect and allow for additional spacing clearances for all underground concrete blocking used for waterlines to support or reinforce horizontal and vertical bends, tees, fire hydrants, air releases, valves and other water facilities. If a contractor or public service provider is uncertain of the presence of any such utilities or facilities, they may contact the City representative for each department listed in Section 3.3(3) or contact the City Right-of-Way Inspector for additional assistance.

3.4 Street Cuts and Excavation

1. The removal of portions of existing pavement, drives, slabs, and sidewalks shall require full depth sawcut by the use of a power-driven saw. Where concrete removal is approved, locations of the removal shown on the plans are indicative only of the need for a sawcut. The removal shall coincide with an existing sawed joint, construction joint, or expansion joint to eliminate overcutting removal shall be to the existing joint. If overcutting past an existing sawed joint, construction joint or expansion joint, is permitted, all concrete overcutting shall be remedied by pressure grouting the entire length of the installation; for asphalt, Texas Department of Transportation approved sealant, as referenced in the Public Works Specifications, shall be used. Any remaining surfaces damaged during removal operations by the contractor shall be replaced to the nearest joint. Concrete and asphalt replacement shall be in accordance with the City's Public Works Standards and Specifications.
2. In the event that it is necessary to place a temporary surface on any cut opening, the temporary surface shall be composed of hot mix asphalt or cold mix paving materials. Gravel or flexbase surface material shall not be used as a temporary surface on any cut. Temporary surfaces shall be adequately compacted to prevent deterioration of repair during the temporary period. Temporary surfaces utilized within a pedestrian accessibility route shall comply with the Americans with Disabilities Act ("ADA"). If temporary surfaces are used within the roadway, driveway or alley, such surfaces shall be smooth with a uniform surface, free of depressions or humps.
3. If a pavement cut is to be covered, the permittee shall use steel plates, or equivalent plates, of sufficient strength and thickness to support all traffic. Plates must be sufficiently secured in place so as not to become dislodged or in any way cause a hazard to traffic. Asphalt transitions must be placed as required to provide a smooth riding surface. Plates must be marked with the name of the person performing the construction and with a local 24-hour contact number that can be used in case of an emergency, unless a sign identifying the contractor is posted at or in close proximity to the work site. Road plates may not be used for more than five (5) working days without prior approval from the City.
4. Any temporary riding surface that deteriorates or fails to meet the requirements of these specifications shall be removed and replaced at the City's discretion and at the permittee's expense.

3.5 Installation

1. Facility Spacing Requirements

- A. Except as provided by Section 3.5(4) of this Manual, all facilities installed under pavement shall be buried to a minimum depth of 48 inches under top of pavement

for the fully improved ultimate roadway width. This measurement shall be made from the existing or proposed top of pavement, whichever is lower. For this section, proposed improvements are defined as any facility with a designated location and elevation as shown on available construction plans. Except as provided by Section 3.5(4) of this Manual, all facilities installed in the parkway shall be buried a minimum of 24 inches. These stipulations are with the condition that additional depth may be necessary due to other constraints or utilities. Requirements for utility structures are provided in Section 5.04 of the Ordinance. Upon written request, an exception may be granted.

- B. All facilities that cross existing drainage facilities, sanitary sewer, or water mains shall either be buried under the existing pipes with a two-foot minimum vertical clearance at the underside of the existing pipes, or be placed above the existing pipes with a two foot vertical clearance at the top of the existing pipe. In either case, the proposed facility must be 48 inches under top of pavement, except as provided by Section 3.5(4) of this Manual. The location and elevation of all crossed existing utilities must be potholed prior to installation of new facility.
- C. All facilities that cross proposed storm sewer, sanitary sewer or water mains shall have a galvanized steel or PVC (not less than Schedule 40) encasement, or approved equivalent, and have a two-foot minimum clearance on any side of the proposed facility. In lieu of the encasement, the facility may be buried five (5) feet below the proposed pipe.
- D. All facilities that run parallel to an existing or proposed drainage facility, sanitary sewer, or water main shall have a three-foot minimum horizontal clearance from the exterior face of the pipes or manholes. Please note that the elevation of the individual lateral services of these pipes may vary. All conduit must be at least two feet below or above, as applicable, all existing or proposed drainage facility, sanitary sewer, or water main. When performing construction in the right-of-way, all facilities shall be located and verified on the same side of the right-of-way as the construction is being performed.

2. Landscape Protection Requirements

The proposed facility route shall be designed to minimize damage to trees or landscaping.

- A. All construction within street medians with trees present must be bored 72 inches below the existing ground. Boring shall enter and exit at least 24 inches outside of the drip line of the tree. The drip line is an imaginary line that extends from the tree's outer branches and leaves, directly to the ground.
- B. If work is performed near a tree, a temporary construction fence shall be erected 12 inches outside the drip line of the tree. The fenced area shall not be entered during the time of construction and the fence shall not be removed for any reason until the work is complete.

- C. Permittee shall be responsible for any damage to public or private landscaping and sprinkler systems.

3. Trenchless Technology/Boring Requirements

In using trenchless technology or boring, the following applies:

- A. Prior to construction, all existing public facilities shall be physically located in the field when crossing over or under water lines, sanitary sewer, or storm drains.
- B. Construction shall be made in such a manner that will minimize interference with vehicular traffic and shall not weaken or damage the existing street.
 - 1. The location of the boring pits shall be a minimum of five feet from the roadway to prevent undermining of the curb, gutter, or shoulder section.
 - 2. The pit shall be dug to a depth sufficient to maintain a minimum boring depth of 48 inches below the traffic surface. Jetting types of boring equipment are not allowed.
 - 3. All overcutting shall be remedied by pressure grouting the entire length of the installation.
 - 4. The pits or trenches excavated to facilitate this operation shall be backfilled and compacted immediately after work is completed.
- C. The contractor shall be able to locate the bore head at all times in accordance with the latest technologies and provide the location of the bore upon request.
- D. All directional boring shall have the locator place bore marks and depths while the bore is in progress. Locator shall place a mark at each stem with a paint dot and indicate the depth at every other stem.

4. Microtrenching

The following standards apply to the construction method of microtrenching.

- A. “Microtrenching” means the process of cutting a trench with a dry cut machine or other applicable equipment and reinstating with like material consistent with the site of the trench or other material approved by the City, for purposes of installing telecommunications facilities.
- B. Microtrenching is only allowed pursuant to a license agreement with the City that is at least thirty (30) years in duration and is for the installation of a telecommunications network providing service to all residential and commercial buildings throughout the City.

- C. Microtrenching is allowed behind the curb in the parkway, or if there is no curb adjacent to a street, microtrenching is allowed in the parkway adjacent to the street. Microtrenching is only allowed in asphalt streets, concrete streets, driveways and sidewalks when approved by the City upon a determination that microtrenching in the parkway is not feasible at a particular location.
- D. Microtrenching is not allowed in concrete streets that are:
1. designated as a thoroughfare, major arterial, minor arterial, major collector, or minor collector by the most current version of the City's Thoroughfare Development Plan; or
 2. adjacent to a section of the public right-of-way that is eleven (11) feet or more behind the curb.
- E. Microtrenching Depth and Reinstatement.
1. Trenches cut using the microtrenching process must be made at a depth of at least twelve (12) inches below the pavement or parkway, as applicable. The City may require trenches cut using the microtrenching process to be made at a depth of not more than sixteen (16) inches below the pavement or parkway, as applicable, if the greater depth will avoid endangering the health, safety, or welfare of the public or property.
 2. Trenches cut using the microtrenching process must be reinstated with like material consistent with the site of the trench or with other material approved by the City.
- F. Location of Trenches.
1. When microtrenching is approved to occur in an asphalt street under Subsection (C), all trenches cut using the microtrenching process must be made in the seam between the street and the gutter pan, in another existing seam or joint in the street, or in an alternate location in the street or gutter that is adjacent to the curb and approved by the City, upon a determination that microtrenching is not feasible in such other locations.
 2. When microtrenching is approved to occur in a concrete street under Subsection (C), all trenches cut using the microtrenching process must be made in an existing seam or joint in the street or in a location in the street or gutter that is adjacent to the curb and approved by the City, upon a determination that microtrenching is not feasible in existing seams or joints.
 3. When microtrenching is approved to occur in a driveway or sidewalk under Subsection (C), all trenches cut using the microtrenching process must be made in existing seams or joints, or in an alternate location approved by the City upon a determination that microtrenching is not feasible in existing seams or joints.

4. Microtrenching is not allowed in driveways that are made of decorative stone, paverstone, or other unique non-concrete or non-asphalt material. Facilities associated with a telecommunications network that are approved to be installed in a location where a driveway described by this Subsection is present must be installed at a depth of at least twenty-four (24) inches below the driveway.
 5. When microtrenching occurs in the parkway within residential areas, all trenches cut using the microtrenching process shall be located directly behind and adjacent to the curb, or as otherwise approved by the City. When microtrenching occurs in the parkway within commercial areas or other non-residential areas, all trenches cut using the microtrenching process shall be located three (3) feet behind the curb, or as otherwise approved by the City.
- G. Construction using the microtrenching process shall be made in such a manner that will minimize interference with vehicular traffic and shall not weaken or damage the existing street. When microtrenching is approved to occur in a concrete street under Subsection (C), a person shall not cut rebar, dowels, or other structural supports within the concrete.
- H. One trench using the microtrenching process may be made on each side of a street. Trenches made using the microtrenching process along a street adjacent to the curb may continue through an intersection and resume at a location along the street adjacent to the curb on the other side of the intersection.
5. Exceptions and variances may be requested in accordance with the Ordinance.

3.6 Backfill

Backfill of all trenches and bore pits shall begin immediately following installation of the new facility in accordance with the attached detail and the following requirements:

1. All loose concrete, rocks, roots, trash and other debris shall be removed from the excavation prior to any backfill being placed.
2. Backfill material shall consist of the native material obtained from the street excavation unless, in the opinion of the City, this material is unsuitable for use.
3. All backfill material shall be compacted in lifts of loose depth not exceeding 8 inches and compacted to at least 95% of Standard Proctor Density at optimum moisture content, \pm two (2) percentage points, as determined by ASTM D698.
4. Instead of backfilling with excavated material, the contractor may backfill with flowable backfill material. In addition, the City may require any entity or contractor to use flowable fill to backfill an excavation in the public right-of-way in the interest of preserving the public convenience or safety. Flowable type backfill shall have a compression strength of 1500 PSI. Flowable backfill material may only be used for excavations in concrete, asphalt, or other paving.

5. The City may perform, or have performed, any material test on compaction. The permittee shall notify the city inspector 24 hours prior to completion of backfill.
6. If the tests on the backfill do not meet the above requirements, the backfill shall be considered unacceptable and shall be removed and replaced. The permittee shall bear the cost of all corrections and subsequent testing if the backfill is deemed unacceptable.

3.7 Restoration

The requirements of this section govern the restoration of public right-of-way surfaces within the City. For those restoration activities not covered here, the applicable provisions of the *Standard Specifications for Public Works Construction - North Central Texas Council of Governments* will govern.

A permittee performing construction in the public right-of-way shall restore the public right-of-way to a condition that is equal to or better than the condition prescribed in this manual or other applicable City design and construction standards. Restoration work must be performed to the satisfaction of the City.

Restoration work to the public right-of-way must include, but is not limited to, the following:

1. Shrubs damaged by the permittee shall be mitigated and offset by planting the same species and size of shrub. Permittees must obtain the approval of the City's Urban Forest and Land Manager regarding any proposed plant material before planting replacement material. Mitigation of canopy and ornamental trees will be assessed by the City Forester and cost of damages will be based on the International Society of Arboriculture's *Guide For Plant Appraisal*. Cost of damages shall be paid to the City.
2. Sod shall be used for turf replacement and shall match existing adjacent type. Ruts shall be removed and the topsoil shall be prepared to provide a smooth surface free of rock and gravel. Irrigation systems shall be repaired to pre-construction condition.
3. The permittee shall comply with the City's Median Landscaping Guidelines and/or the current version of the City's Standard Specifications and Special Provisions for Landscaping for standard details and specifications for landscaping and irrigation repair or replacement. The Standard Specifications are available on the City of Arlington website, www.arlingtontx.gov.
4. Installation or reinstallation of all manholes and handholes shall be completed as required by the City.
5. Backfilling and compaction of all completed bore pits, potholes, trenches, or other holes must be performed on a daily basis, or provide proper protection according

to the Occupational Safety and Health Administration standards.

6. All subgrade, streets, sidewalks and alleys shall be restored as provided in the attached detail.
7. Leveling of all trenches and disturbed areas.
8. Restoration of any damaged traffic control devices must be completed, including but not limited to, imbedded loop detectors, pavement markings, underground conduits and signs.
9. All location flags must be removed during the clean-up process at the completion of the work.
10. Restoration of special street, sidewalk, or drive approach surfaces must be done so that the restoration matches the color, texture, and pattern of the surrounding special surfaces.
11. Restoration must be made in a timely manner. If restoration is unsatisfactory or not performed in a timely manner, then all of the permittee's work on the project in question will be halted, and no additional permit will be issued until the restoration is completed to the satisfaction of the City. Any hold on the permittee's work will include work previously permitted but not completed.

3.8 Exceptions

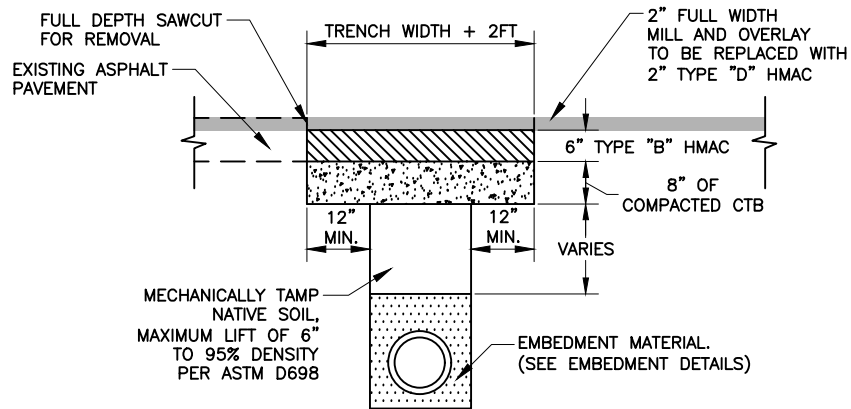
Exceptions to any of these provisions are only allowed if approved by the City in accordance with the Ordinance. Failure to construct facilities in accordance with the ordinance may result in correction of the defects by the City, with all restoration and repair performed at the permittee's expense.

3.9 Miscellaneous

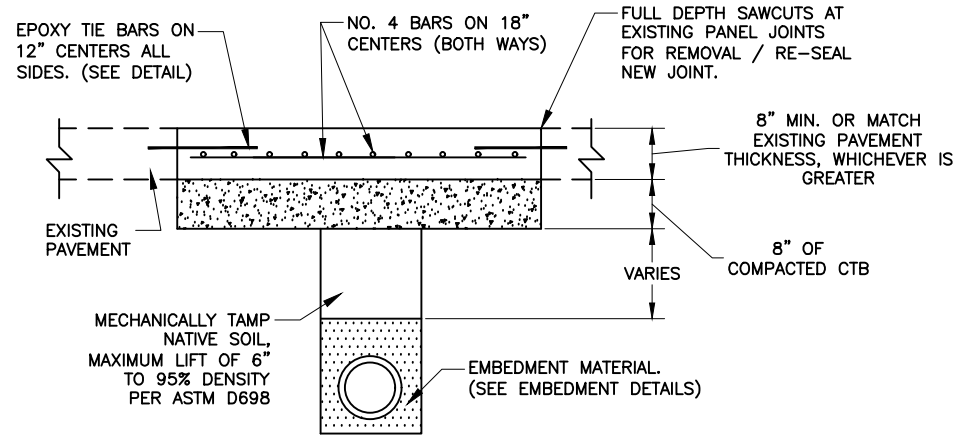
1. Sidewalks. No handhole, vaults, structures, cabinets, or ground boxes may be located in the sidewalk or curb ramp unless approved by the City. All sidewalks shall be backfilled the same day and temporary asphalt shall be placed to restore pedestrian accessibility.
2. Decorative Paving. No decorative pavement or pavers shall be cut, removed or broken without prior approval from the City. Any remediated pavement or pavers shall match the existing pavement.
3. Equipment Identification. All construction equipment and vehicles located and/or stored at the construction site shall have the contractor's company name and number clearly displayed and identified.
4. Above Ground Markers. No above ground markers shall be allowed without approval by the City. Curb markers may be permitted at a location upon approval from the City.

5. Core Holes. Core holes in sidewalks and driveways may be used only to locate utilities and shall be repaired from joint to joint. If necessary, temporary steel plates may be used to protect the void created by the core until the permanent repair is made as long as the plates comply with all accessibility standards under the Americans with Disabilities Act (“ADA”) or other applicable law. If three (3) or more core holes are made under this Section 3.9 within a panel of concrete pavement, the full street panel must be replaced. If three (3) or more core holes are made under this Section 3.9 within a panel of asphalt pavement, the pavement must be restored in accordance with City’s Paving/Drainage/Water/Sewer Standard Details, which is available on the City of Arlington website, www.arlingtontx.gov. Core holes within decorative pavement or pavers of any kind shall not be allowed without prior approval from the City.
6. Dust Control. The public service provider and contractor will be responsible for minimizing dust on a daily basis when cutting or removing pavement, sawing, crack sealing and routing of joints. Dust control measures shall include the use of equipment necessary for the capturing of particulate matter during the process and may include watering stockpiles and subgrade. All equipment used shall be so constructed and operated so that dust or its effect will be minimized by the operations carried on during construction and other work or from anything incident thereto, with respect to persons living in or otherwise present in the vicinity.
7. Cleanup and Staging of Material. The contractor and public service shall daily complete the cleanup and hauling off of trash, unused materials, concrete or other debris. Staging of material and equipment shall be placed in an area away from the traveling public. Additional safety fence and screening may be required when requested by the City to conceal the staging area.
8. Boring. When pulling pipe back after boring, the contractor shall do so from the reel to reduce stringing or placing pipe along the street, sidewalk, in front of driveways or at intersections. If the contractor cannot do so, they shall provide additional notification to the homeowner or occupant of the affected premises if such person(s) cannot access their driveway. Such activity shall not be scheduled around times where high volumes of pedestrian or vehicle traffic normally occur at the location.
9. Removal of Locate Marks. At the request of the City, locate marks must be removed upon completion of the construction process when placed on decorative pavement or pavers, or when placed at any location within:
 - A. The Downtown Business District as defined by Arlington Unified Development Code (“UDC”) § 2.3.8, as amended;
 - B. The Downtown Neighborhood Overlay as defined by UDC § 2.4.3, as amended;
 - C. The Entertainment District Overlay as defined by UDC § 2.4.5, as amended; or
 - D. The Viridian Planned Development as established by Tax Increment Financing (TIF) District #6, as amended.

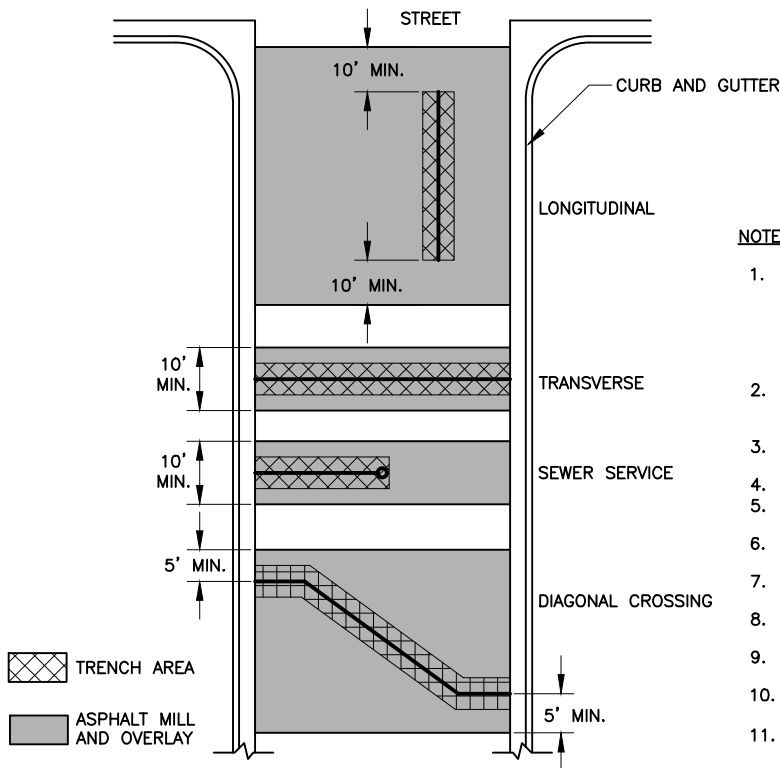
10. Surface and Ground Protection. Tarps or like material shall be used to place excavated soils, rock, stone or sand to sufficiently protect the surface below and prevent the material from migrating to or reaching the surface below. Plywood or equivalent products shall be used to prevent damage or rutting when operating equipment or vehicles on top of pavement and within the public right-of-way.
11. Pipe Restrictions. No single blue pipe or black pipe with a blue stripe shall be installed without prior approval from the City. Exceptions and variances may be requested in accordance with the Ordinance if the pipes are pulled together as part of a package with other pipe colors, so they are distinguished from City water utility infrastructure.
12. Special Events and the Entertainment District.
 - A. Permittees, Public Service Providers, and contractors that plan to do construction work in the Entertainment District are responsible to stay informed of all special events occurring within the Entertainment District that may impact the work, including events involving AT&T Stadium, Choctaw Stadium, Globe Life Field, Esports Stadium Arlington & Expo Center, Arlington Convention Center, and any other major events or venues.
 - B. Clean-up and hauling off of waste materials for construction within the Entertainment District must be completed daily and no materials or equipment shall be left in the construction site overnight unless approved by the Economic Development Services Department, Right-of-Way Division. All open excavations shall be backfilled the day before any event and the City may require that restoration be expedited.
13. Missile and pneumatic boring may only occur with the approval of the City. Certain uses may be considered for approval if they are limited to use under sidewalks and residential driveways less than ten (10) feet wide or other uses that would not interfere with City utilities or facilities.



ASPHALT STREET
NTS



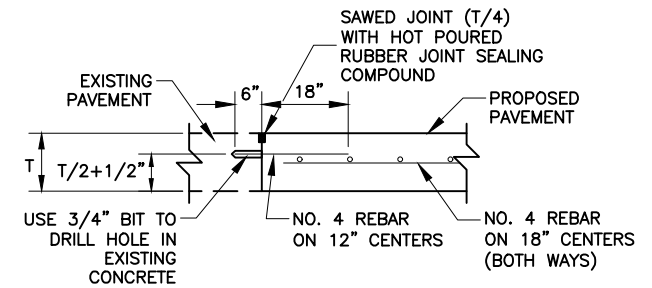
CONCRETE STREET
NTS



TYPICAL ASPHALT
PAVING REPAIRS
NTS

NOTES:

1. A WET SAW SHALL BE USED TO CUT ASPHALT OR CONCRETE FULL DEPTH PRIOR TO OPENING THE DITCH IN ORDER TO INSURE A NEAT STRAIGHT EDGE. BROKEN OR SPALLED EDGES WILL BE RE-SAWED FULL DEPTH BETWEEN JOINTS OR FULL LENGTH OF CUT.
2. CTB - CEMENT TREATED BASE (CONTAINS AGGREGATE) MATERIAL SHALL BE MECHANICALLY TAMPED TO 95% DENSITY PER ASTM D698.
3. ALL CONCRETE PAVEMENT SHALL BE CLASS C, 6 SACK, 3600 PSI.
4. ALL CONCRETE PAVEMENT MUST BE VIBRATED.
5. A 10' STRAIGHT EDGE WITH HANDLE SHALL BE USED ON ALL CONCRETE PANELS.
6. ALL CONCRETE PAVEMENT MUST BE BAKER BROOM FINISHED.
7. WHITE PIGMENT CURING COMPOUND MUST BE USED FOR ALL CONCRETE PAVEMENT.
8. ALL MATERIALS AND CONSTRUCTION TO MEET CITY OF ARLINGTON STANDARD SPECIFICATIONS.
9. ALL CONCRETE STREET REPAIRS SHALL BE THE FULL PANEL REPLACEMENT.
10. ALL ASPHALT STREET REPAIRS SHALL BE FULL-WIDTH (CURB TO CURB).
11. TRANSVERSE TRENCHES THAT CROSS THE ENTIRE ROADWAY SHALL BE PERFORMED BY METHOD OTHER THAN OPEN CUT IF POSSIBLE.
12. MILL & OVERLAY WILL ONLY BE ALLOWED IF THE EXISTING HMAC IS A MINIMUM OF 6" AND THE OVERALL CONDITION INDEX (OCI) IS GREATER THAN 50. IF THE OCI IS LESS THAN 50, RECLAMATION MUST BE PERFORMED. SEE RECLAMATION DETAILS.



EPOXY TIE-BAR (DETAIL)
NTS

PERMANENT BACKFILL & REPAIR
NTS
REV: 4/9/20



CITY OF ARLINGTON, TEXAS

PERMANENT BACKFILL & REPAIR

DATE:	SCALE: NTS	SHEET: OF
DESIGNED BY:	DRAWN BY:	CHECKED BY: