

# Enhance Regional MOBILITY

Q4 UPDATE FY2014

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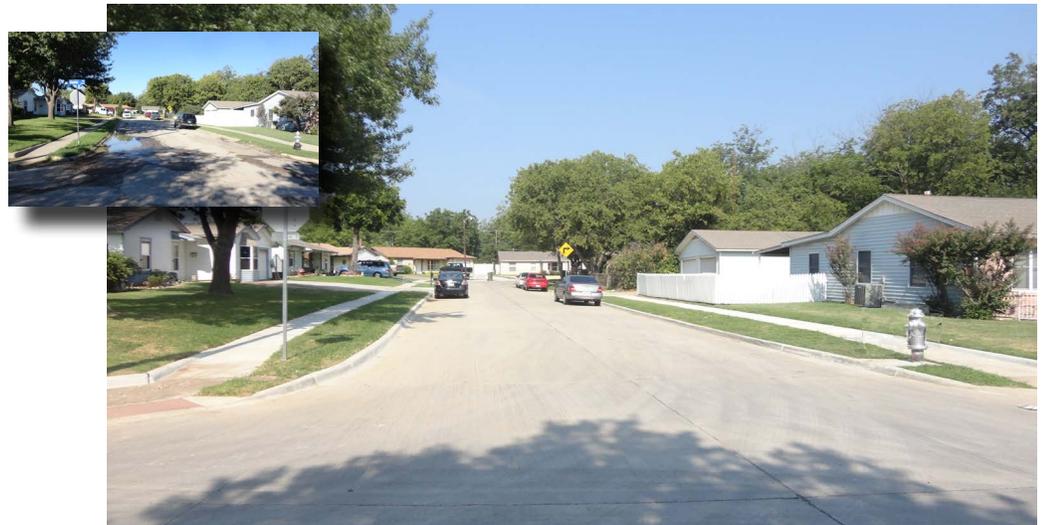
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## UTA students win contest for innovation

Three UT Arlington students won \$10,000 in the NTx Apps Challenge, which aimed to solve regional challenges. GridLock, developed by Zedd Shmais, James Staud, and Nhat Tran, is a real-time monitoring system that analyzes traffic conditions and enables better vehicular flow.

The group plans to refine their app and bring it to life, making it available to drivers in North Texas.

This story was originally printed on MyArlingtonTX, and can be read in full at [www.arlington-tx.gov/news](http://www.arlington-tx.gov/news).



## Grant funds contribute to City infrastructure

The City of Arlington combines multiple resources to fund the Street Maintenance Program across the City. Recently, the City used Community Development Block Grant (CDBG) funds in combination with Water and Sewer Bond funds to complete the second of a three-phase program to improve some of

the worst streets and water/sewer lines in East Arlington. Federal grant funds such as CDBG allow local funding to stretch farther. This project in particular is an important step City's efforts to revitalize East Arlington. Not only do the streets look better, but residents are assured that the updated water

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# Highlights

## Abram Street Downtown makeover

This spring, the City worked with residents and local business owners to determine a concept for the rebuild of Abram Street between Collins Street and Cooper Street. City Council approved city staff to move forward with designing a road that is narrowed to two through-travel lanes and a center turn lane through Downtown Arlington. This project supports the 2004 Downtown Arlington Master Plan, and funding was approved by voters in the 2008 Bond Program. Comments and feedback from public meetings indicated support for a street with pedestrian elements, including wider sidewalks and benches.

Since City Council approved the project this summer, city staff has identified the design firm, Schrickel, Rollins and Associates, Inc. to make the concept a reality.

The design process is expected to take one year, followed by a year of right-of-way acquisition and utility relocation, at which point the City can begin construction in the 2016/2017 timeframe.

One of the most innovative proposed design elements is called “reverse-angle back-in” parking, which is much easier than parallel parking and much safer than front-in angle parking since you can see the street when you are pulling out. You can see how this works by checking out this YouTube video: [www.youtube.com/watch?v=cSsdl-dew9E](http://www.youtube.com/watch?v=cSsdl-dew9E)

For more information and updates about this project, please visit: [www.arlingtontx.gov/publicworks/abramstreet.html](http://www.arlingtontx.gov/publicworks/abramstreet.html).



## Grant-funded streets, cont.

and sewer systems will function properly for years to come.

Phase 1 was completed in 2013, and consisted of:

- Southmoor Drive
- Jeanette Court
- Elaine Court
- LeJuan Court
- Laura Lane

Phase 2 was completed in October of 2014 and included:

- Lackland Street from Park Row to Daniels
- Lovers Lane from Collins to Wynn Terrace

Phase 3 is planned to include:

- Kimberly Drive from Mignon to Sherry Street
- Barton Drive from Kent to Sherry

When grant-funded infrastructure projects are combined with other grant-funded efforts such as housing rehabilitation, neighborhood parks, community centers, code compliance, and neighborhood beautification, the City can make a big impact in our goal to make East Arlington a place that residents love to live.

2014 COMPLETED PROJECTS * Street Improvements in conjunction with a Water/Sewer Renewal or Drainage Project	2013				2014				2015			
	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4
	Arbrook/Melear Drainage Reclamation (portion of streets only)*											
Birch Street (McKinney - Woodland) Reclamation*												
Bowman Springs Road (IH 20 - South City Limits) Reconstruction												
Britannia Gardens Subdivision Reclamation*												
Daniel Drive (Park Row - Lovers) Reconstruction												
Fairfield Subdivision Reclamation*												
Lackland Street (Park Row - Daniel) Reconstruction												
Lake Country Drive (Stonebrook - Dead end) Reclamation*												
Lavender Lane (Weymouth Ct - Winewood) Reclamation*												
Lovers Lane (Daniel - New York) Reconstruction												
Lovers Lane (Collins - Wynn Terr) Reconstruction												
McKinney Street (Randol Mill - Sanford) Reclamation*												
Sanford Street (McKinney - Orange) Reclamation*												
S. Cooper/Mayfield Road Intersection - Adding Turn Lanes												
S. Cooper/Green Oaks Intersection - Adding Turn Lanes												
Stonebrook Drive (Lamar - Lake Country) Reclamation*												
Thousand Oaks Subdivision Reclamation*												
Weymouth Ct (Lavender - North dead end) Reclamation*												
Wright Street (McKinney - Forest) Reclamation*												

CURRENTLY UNDER CONSTRUCTION * Street Improvements in conjunction with a Water/Sewer Renewal or Drainage Project	2014				2015				2016			
	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4
	Abram Street (Stadium - SH 360) Reconstruction											
Abram Street (SH 360-City Limits) Reconstruction												
Abram Street (Bowen - Norwood Place) Reclamation*												
Blackwood Drive (Bowen - Norwood) Reclamation*												
Blue Forest Drive (Harris - Ledbetter) Reclamation												
Bowen Road Northbound Turn Lane at IH20 Construction												
Bradford Drive (Sherry - Kent) Reconstruction												
Calender Road (Harris - Monthaven) Reconstruction												
Cedar Street (Davis - East end) Reclamation*												
Cedar Street (West end - Magnolia) Reclamation*												
Center Street (Arkansas - Timberview) Reconstruction												
Center/IH 20 Bridge (TxDOT Project) Construction												
College Street (Abram - UTA Blvd) Reclamation*												
Cooper Street (Pleasant Place - Mitchell) Sidewalks & Median												
Crestridge Court (East end - Tucker) Reclamation*												
Curry Road (Winthorp - Blake) Reconstruction												
Front Street (Pecan - Oak) Parking Lot												
Harris Road (Calender - 300' east of Calender) Reconstruction												
Hollandale Circle (SH 360 - Hardy Place) Reconstruction												
Inwood Drive (Meadow - Southridge) Reclamation*												
Lamar Blvd (Collins - Ballpark Way) Reconstruction												
Ledbetter Road (Curry - Russell) Reconstruction												
Napier Drive (Green Oaks - East dead end) Rebuild*												
Norwood Ln (West end - 2030 Norwood Lane) Reclamation*												
Pamela Lane (SH 360 - Plaza) Reconstruction												
Parker Heights Subdivision Reclamation*												
Penny Royal Court (West dead end - East dead end) Reclamation*												
Royce Drive (Arkansas - Napier) Rebuild*												
Russell Road (Ledbetter - Calender) Reconstruction												
San Ramon Village Reconstruction												
Southridge Drive (Inwood - Tucker) Reclamation*												
Spring Meadows Subdivision Reclamation*												
Summit Street (Abram - Fourth St) Reclamation*												
Tate Springs Road (Oak Branch - Green oaks Blvd) Reclamation*												
Truver Lane (Green Mere - North dead end) Reclamation												

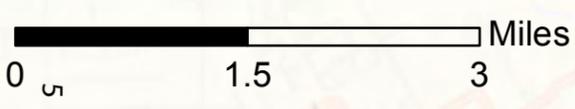
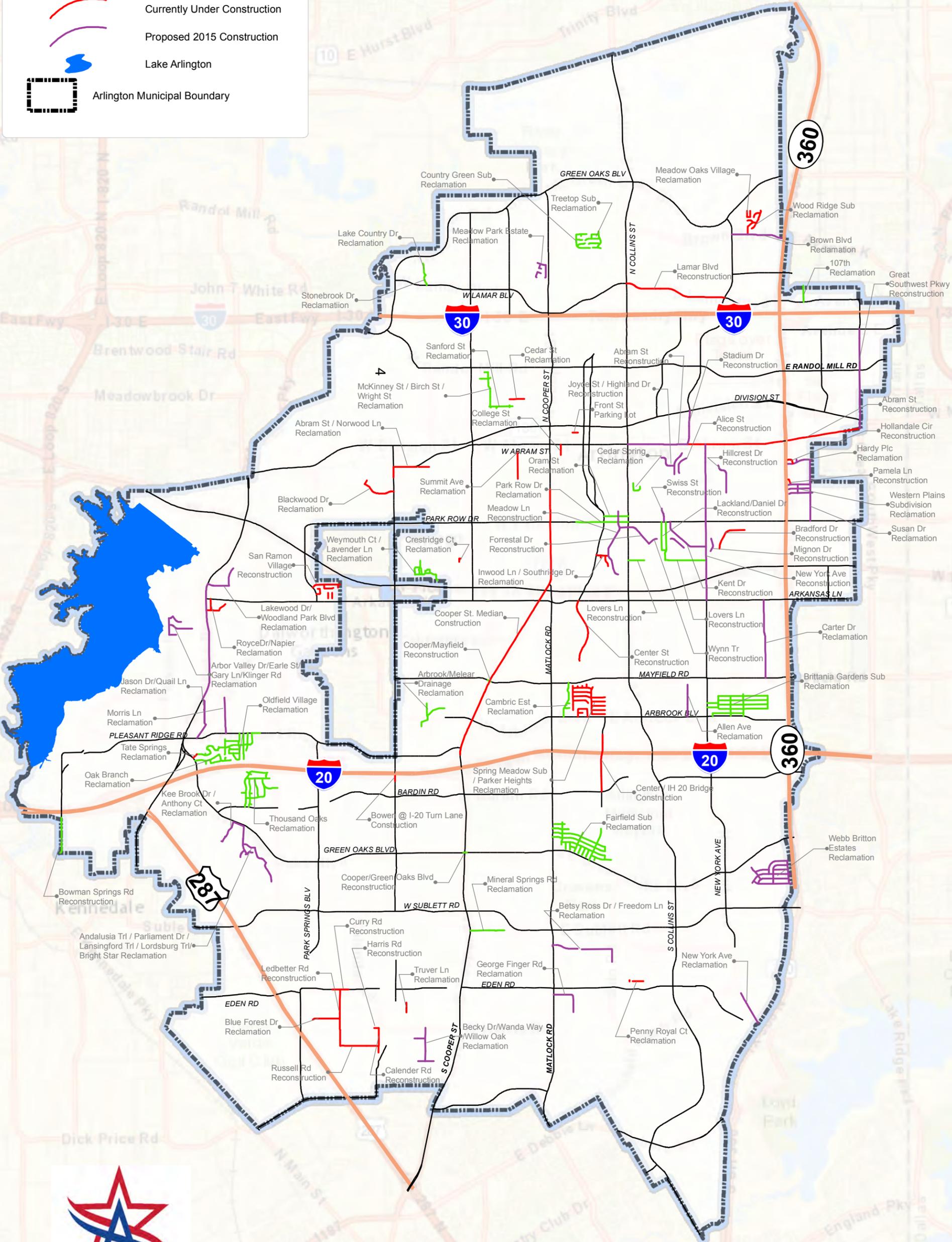
2014 STREET MAINTENANCE PROJECTS No design or right-of-way are required for the annual street maintenance projects. ** Street Reclamation Under Construction Completed	2014			
	Q1	Q2	Q3	Q4
	107th Street (Ave H - North City Limits)**			
Cambric Estates**				
Cedar Springs Terrace (Mitchell - Mitchell)**				
Country Green Subdivision**				
Meadow Oaks Village Subdivision**				
Mineral Springs Road (Cooper - Tempest)**				
Oak Branch Estates**				
Oldfield Village Subdivision**				
Oram Street (Center - Collins)**				
Park Row Drive (Center - Collins)**				
Treetop Subdivision **				
Wood Ridge Subdivision**				



**LEGEND**

**Status**

-  2014 Completed Projects
-  Currently Under Construction
-  Proposed 2015 Construction
-  Lake Arlington
-  Arlington Municipal Boundary



Quarters are based on calendar year.

**PROPOSED 2015 CONSTRUCTION**

-  Design Phase
-  Right-of-way Phase
-  Construction Phase (includes franchised utility relocation)
- \* Street Improvements in conjunction with a Water/Sewer Renewal or Drainage Project

	2015				2016				2017			
	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4
Abram Street (Collins - Stadium) Reconstruction												
Alice Street (Browning - Sharon) Reconstruction												
Andalusia Trail (Parliament - Lansingford) Reclamation*												
Antony Court (Kee Brook - Andalusia) Reclamation*												
Arbor Valley Drive (2701 Shelterwood - Perkins) Reclamation*												
Betsy Ross Drive (Cul-de-sac - Silo) Reclamation *												
Bright Star Trail (Parliament - Rimrock) Reclamation*												
Brown Blvd (Ballpark Way - SH 360) Reclamation*												
Earle Street (Perkins - Highgate) Reclamation*												
Forrestal Drive (Meadow - Wynn Terr) Reconstruction												
Freedom Lane (Liberty - Pilgrim) Reclamation*												
Gary Lane (Perkins - Klinger) Reclamation*												
Great Southwest Parkway (Avenue E - Abram) Reconstruction												
Hardy Place (Susan - SH 360) Reclamation*												
Highland Drive (Sharon - Ruth) Reconstruction												
Hillcrest Drive (Glenhaven - Brookshire) Reconstruction												
Jason Drive (Quail - Little) Reclamation*												
Joyce Street (Meadowbrook - Highland) Reconstruction												
Kee Brook Drive (Green Oaks - Antony Ct) Reclamation*												
Kent Drive (Park Row - New York ) Reconstruction												
Klinger Road (Gary - 5806 Klinger) Reclamation*												
Lakewood Drive (Treatment Plant - Woodland Park) Reclamation*												
Lansingford Trail (Andalusia - Lordsburg) Reclamation*												
Lordsburg Trail (Lansingford - cul-de-sac) Reclamation*												
Meadow Lane (Tharp - Rosewood) Reconstruction												
Meadow Park Estates Reclamation *												
Mignon Drive (South dead end - Pavement Change) Reconstruction												
New York Ave (Park Row - Arkansas) Reconstruction												
Parliament Drive (Green Oaks - Andalusia) Reclamation*												
Parliament Drive (Bright Star - Aurora Ct) Reclamation*												
Quail Lane (Green Oaks - Jason) Reclamation*												
Stadium Drive (Division - Abram) Reconstruction												
Susan Drive (Hardy Place - Park Row) Reclamation*												
Swiss Street (Moore - Park Row) Reconstruction												
Western Plains Subdivision Reclamation*												
Woodland Park Blvd (Lakewood - Green Oaks) Reclamation*												
Wynn Terrace (Forrestal - Biggs) Reconstruction												
Wynn Terrace (Arbor - Browning) Reconstruction												

**2015 STREET MAINTENANCE PROJECTS**

- No design or right-of-way are required for the annual street maintenance projects.
- \*\* Street Reclamation
-  Proposed Construction

	2015			
	Q1	Q2	Q3	Q4
Allen Avenue(Arbrook - Maybrook)**				
Becky Drive (Willow Oak - West end)**				
Carter Drive (Arkansas - Mayfield)**				
George Finger Road (Matlock - Katydid Ct)**				
Morris Lane (Little - Pleasant Ridge)**				
New York Avenue (Duvall - SH 360)**				
Wanda Way (Hamlet - 1807 Wanda Way)**				
Webb Britton Estates**				
Willow Oak Lane (Harris - Wanda Way)**				

# Updates

## 2014 Bond Election passes

On November 4, 2014, voters turned out to approve all propositions of the bond package, with 75% of voters voting for Proposition 1 – Streets. Over \$160 million of the \$236 million bond package will allow the City to continue improving roadways for drivers in Arlington. In addition to roadways improvements, the Parks and Recreation proposition will include trails and pedestrian bridges at several parks throughout the City, including River Legacy Park. Major projects to be funded by the bond package include:

- \$40,150,000 for the Residential Rebuild Program
- \$15,000,000 for Eden Road (between US 287 and Calender)
- \$14,400,000 for Matlock Road (between Bardin and Green Oaks)
- \$13,655,000 for Turner Warnell Road (between Matlock and Cooper)
- \$11,000,000 for Collins (between SH 303/Pioneer Parkway and Park Row)

For a complete list of projects included in the 2014 Bond Package, visit [www.arlington-tx.gov/bond](http://www.arlington-tx.gov/bond).



## Center Street Bridge

Construction has begun on the Center Street Bridge over Interstate-20. The \$10.4 million project with TxDOT will improve mobility, safety, and connectivity, and is expected to be complete summer 2016. The project will require some road closures, including I-20. You can monitor current road conditions throughout the project at [www.drivetexas.org](http://www.drivetexas.org).

*The additional safety feature allows us to do our jobs safer and helps to assure us that the people around us are safer, too*

Motor Officer Cliff Elliot,  
@ArlingtonPD

Be a part of the conversation on social media @CityofArlington:

**#EnhanceRegionalMobility**



## APD Motors Unit Leads Safety on Roadways

Arlington Police Department has recently been able to install light safety bars on all 18 Police motorcycles, due to generous donations from the Arlington Police Foundation, the Citizens Police Academy Alumni Association and Ray Davis, an American businessman and part owner of the Texas Rangers Major League Baseball Team.

The improvements are useful to caravan escorts and traffic stops, since motor officers are now up to three times more visible than before. The newly retrofitted light bars are located about 15 inches above the existing red and blue LED emergency lights, enhancing safety for the motorcycle officer, the driving public, and the millions of tourists who make North Texas their destination every year for entertainment and family fun.

# MAX By The Numbers

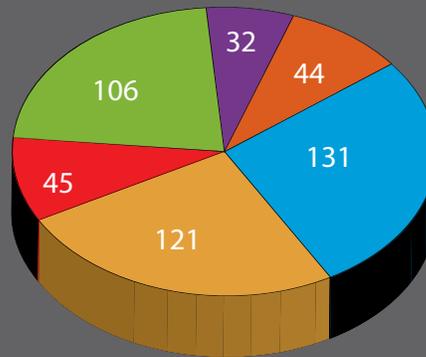
In September 2014, the City conducted a second on-board survey collect feedback from MAX riders. Information collected will be used to inform future decisions the City may make regarding MAX. The City received 239 survey responses. The graphs and information included here illustrate some of the information received.



## Who Rides MAX?

- 58.6% of riders are male; 41.1% female
- 46.4% are between the ages of 18 to 24 years old
- 41.6% are students; 29.2% are employed full-time
- 51.9% use MAX to get to school

## What service (if added) would allow you to ride more often?



- More Stops
- Longer Hours
- More Frequent Service
- Saturday Service
- Special Event Service
- Other

## Rate your experience on MAX



## What would make MAX better?

The September 2014 survey revealed that almost half of the riders ride MAX 5 days a week. Once again, we asked the riders how to make MAX better.

- 131 respondents would like to see Saturday Service
- 121 respondents would like to see more frequent service
- 85 respondents would like to have a stop at Parks Mall
- 56 respondents would like to have a MAX stop at UTA on the west side of Cooper Street